

Chapter 1

Introduction and background



1 Introduction and background

1.1 Proponent

Transport for NSW
231 Elizabeth Street
Sydney NSW 2000
ABN: 18 804 239 602

1.2 Introduction

Transport for NSW (Transport) proposes to construct and operate the second stage of Parramatta Light Rail (the project). The project would connect the Parramatta CBD and the first stage of Parramatta Light Rail to Camellia, Rydalmere, Ermington, Melrose Park, Wentworth Point, Sydney Olympic Park, and the Carter Street precinct in Lidcombe, adjacent to Sydney Olympic Park. Most of the project is located in the City of Parramatta local government area (LGA). A small section is located in the City of Ryde LGA.

The project is subject to assessment under Part 5, Division 5.2 of the *Environmental Planning and Assessment Act 1979* (EP&A Act) and approval by the Minister for Planning.

The project also requires approval by the Australian Government Minister for the Environment and Water under the Commonwealth *Environment Protection and Biodiversity Conservation Act 1999* (EPBC Act).

An environmental impact statement (EIS) was prepared to assess the potential impacts of the project, and to identify the management measures to address those impacts. The EIS was exhibited by the NSW Department of Planning and Environment from 9 November 2022 to 16 December 2022.

1.3 The project as described in the EIS

The project described and assessed in the EIS comprises two main elements:

- construction of about 10 kilometres of light rail infrastructure between Camellia and the Carter Street precinct adjacent to Sydney Olympic Park
- operation of about 13 kilometres of light rail alignment between the Parramatta CBD and the Carter Street precinct, including a section of infrastructure constructed by Parramatta Light Rail Stage 1 between Camellia and the Parramatta CBD.

The key features of the exhibited project, which are shown on Figure 1.1 of the EIS, include:

Light rail track and bridges

- a new 10 kilometre long dual light rail track, with 14 stops, between Camellia and the Carter Street precinct adjacent to Sydney Olympic Park
- two bridges over the Parramatta River between Camellia and Rydalmere, and between Melrose Park and Wentworth Point
- a bridge over Silverwater Road between Rydalmere and Ermington
- other bridge works in Ken Newman Park and Sydney Olympic Park.

Active and public transport integration

- about 8.5 kilometres of new active transport links between Camellia and the Carter Street precinct, which would connect with the existing cycling and pedestrian network
- interchanges with other forms of public transport, including trains, ferries, buses and Sydney Metro West, with the main interchanges located in the Parramatta CBD, Rydalmere and Sydney Olympic Park
- a shared light rail and pedestrian zone (no through vehicle access) within Sydney Olympic Park along Dawn Fraser Avenue between Australia Avenue and Olympic Boulevard
- bus access over the proposed bridge between Melrose Park and Wentworth Point.

Other works

Works proposed to support the project's operation:

- turnback facilities, including along part of Macquarie Street in the Parramatta CBD
- adjustments to the Parramatta Light Rail stabling and maintenance facility at Camellia
- five new traction power substations to convert electricity to a form suitable for use by light rail vehicles
- new and improved open spaces and recreation facilities at Ken Newman Park, the Atkins Road stop and Archer Park.

The timing of the opening of the project is subject to planning approval and the completion of detailed design. Subject to planning approval and procurement, construction is planned to commence in 2025 and the first passenger services are anticipated to commence from 2030/31.

A description of the exhibited project is found in chapters 6 (Project description – infrastructure and operation) and 7 (Project description – construction) of the EIS prepared by Transport in November 2022 (available at: [Parramatta Light Rail Stage 2 EIS](#)).

1.4 Overview of the proposed amendments and refinements

In accordance with section 179(2) of the Environmental Planning and Assessment Regulation 2021 (the EP&A Regulation) an application for approval of a State significant infrastructure project may, with the approval of the Planning Secretary, be amended before it is determined. During and subsequent to public exhibition of the EIS, Transport has undertaken further investigations and is proposing a number of design amendments and refinements. The aim of these amendments/refinements is to address issues raised during engagement and in submissions, take into account further design development, and minimise the potential impacts of the project where practicable.

The proposed amendments are summarised in Table 1.1. Further information about the proposed amendments is provided in Chapter 4 (Description of the amendments) of this report.

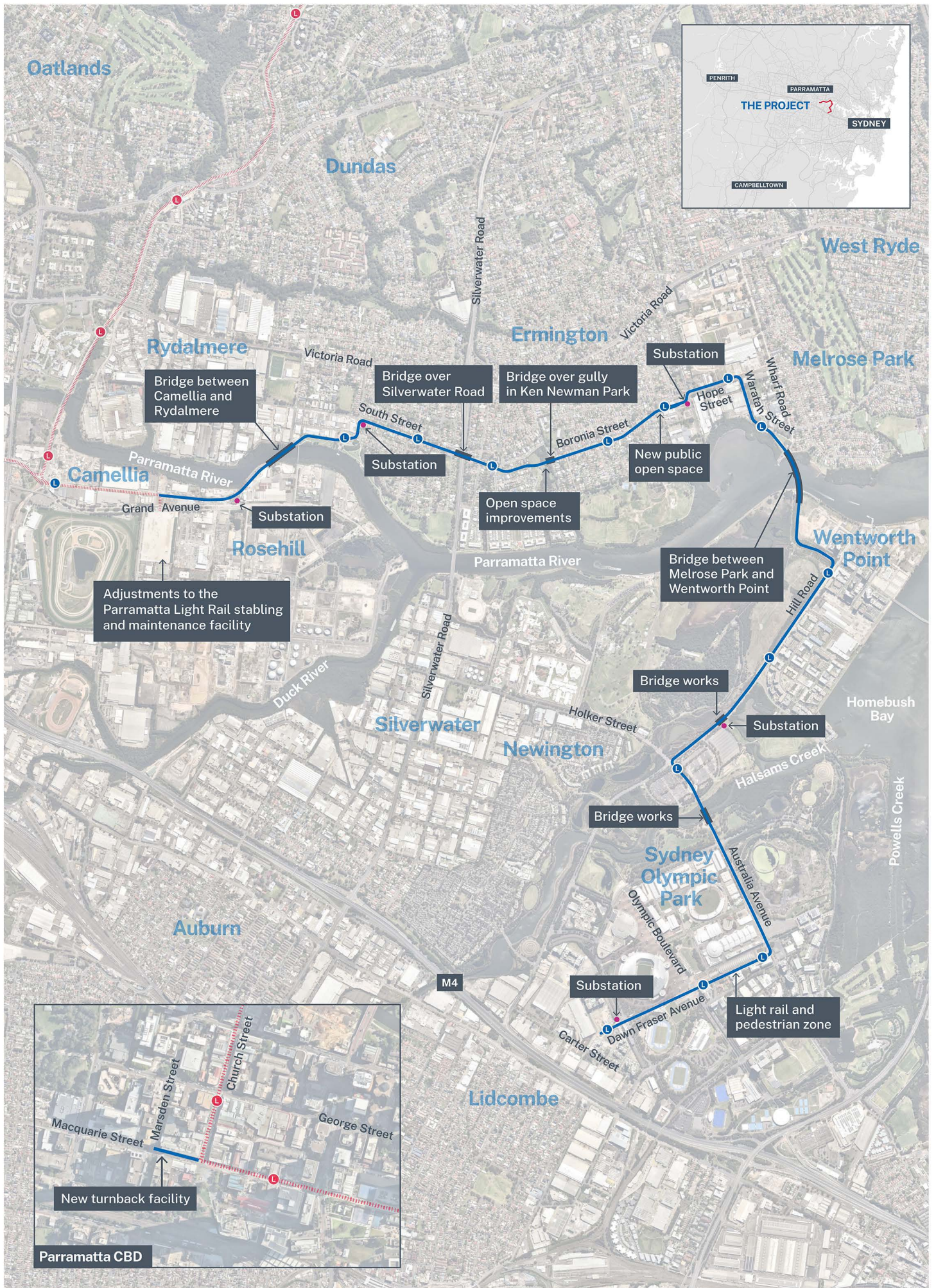
Table 1.1 Summary of proposed amendments

Proposed amendment or refinement	Overview
Camellia foreshore to Rydalmere alignment and bridge	<p>As described in section 5.4.2 and Appendix D of the EIS, investigation of an alternative alignment between Camellia and Rydalmere (the ‘Camellia foreshore to Rydalmere option’) was ongoing in parallel with development of the EIS. It is now proposed to amend the project to incorporate this alternative alignment of the light rail track, active transport link and bridge over the Parramatta River.</p> <p>The new alignment extends along the Sandown Line corridor in Camellia; however, instead of crossing south over to Grand Avenue, it continues along the Parramatta River foreshore in Camellia before extending across a new bridge structure and along the boundary of Eric Primrose Reserve in Rydalmere.</p> <p>The bridge design has been amended, and includes different pier arrangements in the river. It is also proposed to locate the light rail stop at John Street closer to Rydalmere Wharf.</p>
Bridge between Melrose Park and Wentworth Point	<p>The project as described in the EIS included a bridge located between the southern end of Wharf Road in Melrose Park and the northern end of Wentworth Point. It is proposed to amend the alignment and locate the bridge further to the west to avoid direct impacts to residential properties. The works would also include removing the existing high voltage transmission tower at Melrose Park and relocating the wires to three new poles located to the west of the original tower.</p>
Bridge at Hill Road	<p>The project as described in the EIS included retaining the Hill Road bridge in Sydney Olympic Park and providing a new bridge for light rail vehicles on the western side of the existing bridge.</p> <p>It is now proposed to remove the existing bridge at Hill Road and construct a new bridge, which would accommodate road traffic and light rail vehicles in an on-road (segregated) running corridor to reduce impacts on Narawang Wetland.</p>

In addition, refinements are proposed to the location of the traction power substation near Atkins Road, and the cut and fill volumes generated during earthworks.

The project description chapters provided in the EIS (chapters 6 and 7) have been updated taking into account the proposed amendments/refinements. The amended project description (comprising updated chapters) is provided in Appendix A (Updated project description) of this report.

The location and key features of the project (as amended) are shown on Figure 1.1.



LEGEND

- Proposed project alignment
- - - Parramatta Light Rail Stage 1 alignment
- Proposed project stops
- Parramatta Light Rail Stage 1 stops

Figure 1.1 Key features of the project (as amended)

0 1km



1.5 Purpose and structure of this amendment report

Transport has prepared this Amendment Report for the purposes of section 179 of the EP&A Regulation and having regard to Department of Planning and Environment's [State Significant Infrastructure and State Significant Project Guidelines \(DPIE, 2021a\)](#).

The purpose of an amendment report is to describe the proposed amendments and assess the potential impacts of the amended project. The EIS provides an assessment of the potential impacts of constructing and operating the exhibited project. This report considers whether the proposed amendments would result in any changes to the impacts described by the EIS, and whether any changes to the mitigation measures are required. It has been prepared by undertaking a screening-level environmental impact assessment supported by additional impact assessments for key issues where potential changes to impacts have been identified (mainly in relation to noise, biodiversity, flooding and heritage).

The structure of the amendment report is outlined in Table 1.2.

Table 1.2 Structure of this report

Chapter	Description
Chapter 1	Introduction and background (this chapter) Provides a summary of the project as described in the EIS and an overview of the proposed amendments.
Chapter 2	Approval framework Provides an overview of the statutory context for the project (including the proposed amendments) and the next steps in the approval process.
Chapter 3	Strategic context and need Provides an overview of the strategic context and need for the project (including the proposed amendments) and any changes from the EIS.
Chapter 4	Description of the amendments Provides a description of, and justification for, the proposed amendments.
Chapter 5	Stakeholder and community engagement Provides an overview of the engagement process that has been carried out for the proposed amendments.
Chapter 6	Additional environmental assessment Describes the assessment of the potential impacts of the amended project.
Chapter 7	Justification of amended project and conclusion Provides a justification and evaluation of the amended project as a whole, having regard to its environmental and social impacts and the principles of ecologically sustainable development.
Chapter 8	References