

Chapter 1

Introduction and background



1. Introduction and background

1.1 Proponent

Transport for NSW
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1.2 Introduction

Transport for NSW (Transport) proposes to construct and operate the second stage of Parramatta Light Rail (the project). The project would connect the Parramatta CBD and the first stage of Parramatta Light Rail to Camellia, Rydalmere, Ermington, Melrose Park, Wentworth Point, Sydney Olympic Park, and the Carter Street precinct in Lidcombe, adjacent to Sydney Olympic Park. Most of the project is located in the City of Parramatta local government area (LGA). A small section is located in the City of Ryde LGA.

The project is subject to assessment under Part 5, Division 5.2 of the *Environmental Planning and Assessment Act 1979* and approval by the Minister for Planning.

The project also requires approval by the Australian Government Minister for the Environment and Water under the Commonwealth *Environment Protection and Biodiversity Conservation Act 1999* (EPBC Act).

An environmental impact statement (EIS) was prepared to assess the potential impacts of the project, and to identify the management measures to address those impacts. The EIS was exhibited by the NSW Department of Planning and Environment from 9 November 2022 to 16 December 2022.

On 21 December 2022 the Planning Secretary requested Transport submit a response to the issues raised in submissions to the EIS in accordance with section 5.17(6)(a) of the EP&A Act. This Response to Submissions identifies and responds to the issues raised during the public exhibition of the EIS.

Following the exhibition of the EIS, Transport refined the reference design for the project and a number of amendments are proposed. A separate Amendment Report has been prepared to describe and assess the potential impacts of the proposed amendments and to identify how those impacts would be managed and mitigated.

1.3 Overview of the project

The project described and assessed in the EIS comprises two main elements:

- construction of about 10 kilometres of light rail infrastructure between Camellia and the Carter Street precinct adjacent to Sydney Olympic Park
- operation of about 13 kilometres of light rail alignment between the Parramatta CBD and the Carter Street precinct, including a section of infrastructure constructed by Parramatta Light Rail Stage 1 between Camellia and the Parramatta CBD.

The key features of the amended project, which are shown on Figure 1.1, include:

Light rail track and bridges

- a new 10 kilometre long dual light rail track, with 14 stops, between Camellia and the Carter Street precinct adjacent to Sydney Olympic Park
- two bridges over the Parramatta River between Camellia and Rydalmere, and between Melrose Park and Wentworth Point
- a bridge over Silverwater Road between Rydalmere and Ermington
- other bridge works in Ken Newman Park and Sydney Olympic Park.

Active and public transport integration

- about 9.5 kilometres of new active transport links between Camellia and the Carter Street precinct, which would connect with the existing cycling and pedestrian network
- interchanges with other forms of public transport, including trains, ferries, buses and Sydney Metro West, with the main interchanges located in the Parramatta CBD, Rydalmere and Sydney Olympic Park
- a shared light rail and pedestrian zone (no through vehicle access) within Sydney Olympic Park along Dawn Fraser Avenue between Australia Avenue and Olympic Boulevard
- bus access over the proposed bridge between Melrose Park and Wentworth Point.

Other works

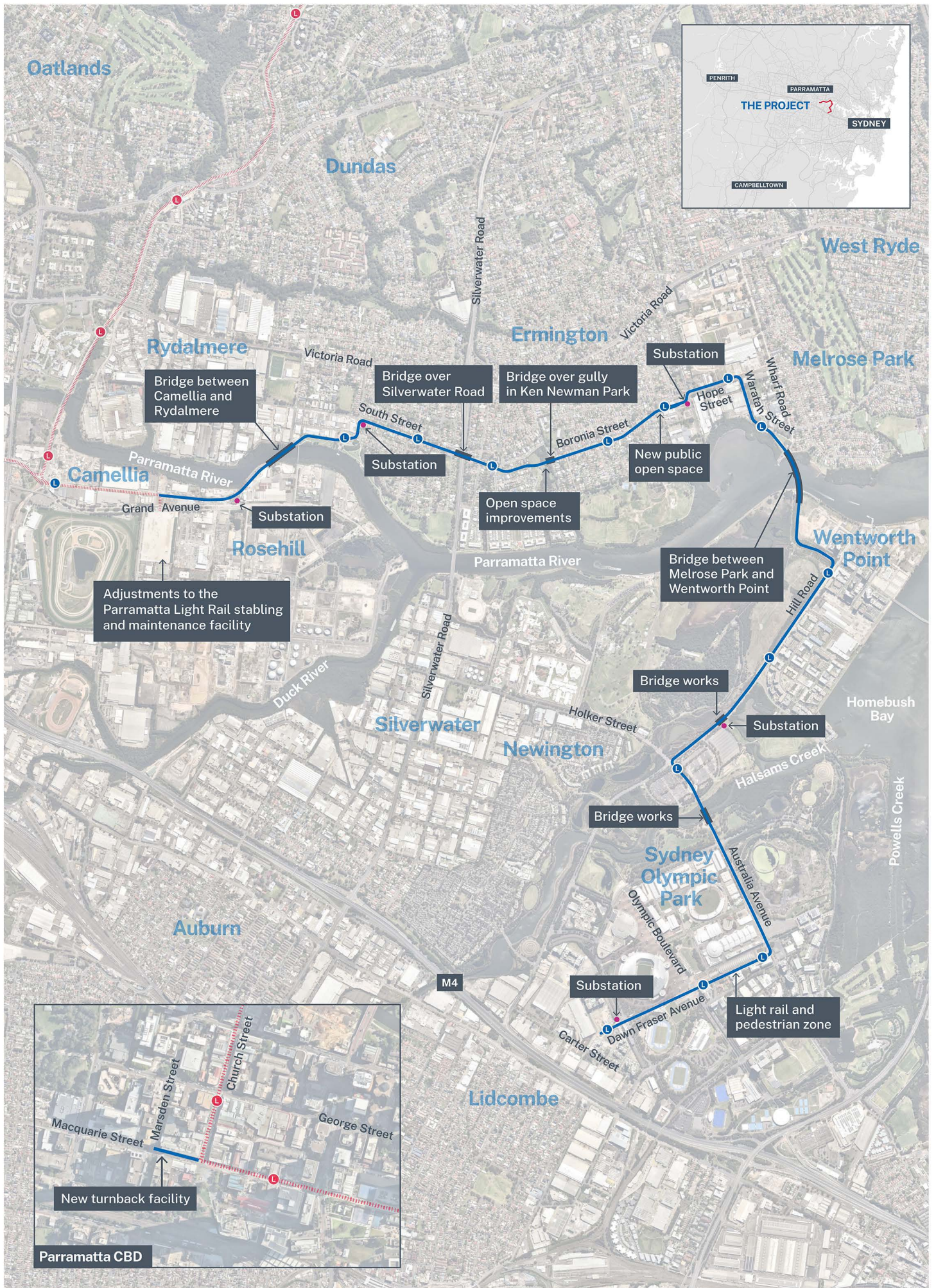
Works proposed to support the project's operation:

- turnback facilities, including along part of Macquarie Street in the Parramatta CBD
- adjustments to the Parramatta Light Rail stabling and maintenance facility at Camellia
- five new traction power substations to convert electricity to a form suitable for use by light rail vehicles
- new and improved open spaces and recreation facilities at Eric Primrose Reserve, Ken Newman Park and the Atkins Road stop.

The timing of the opening of the project is subject to planning approval and the completion of detailed design. Subject to planning approval and procurement, early work is planned to start in 2024, with main construction starting in 2025. First passenger services are anticipated to commence from 2030/31.

A description of the exhibited project is found in Chapters 6 (Project description – infrastructure and operation) and 7 (Project description – construction) of the EIS prepared by Transport in November 2022 (available at: [Parramatta Light Rail Stage 2 EIS](#)).

An updated description of the project, incorporating the proposed amendments, is provided in Appendix A of the Amendment Report, which is available separately.



LEGEND

- Proposed project alignment
- - - Parramatta Light Rail Stage 1 alignment
- Proposed project stops
- Parramatta Light Rail Stage 1 stops

Figure 1.1 Key features of the project (as amended)

0 1km



1.4 Purpose and structure of this report

Transport has prepared this Response to Submissions to address the Planning Secretary's request to submit a response to the issues raised in submissions to the EIS as received following completion of public exhibition on 16 December 2022 and the Department of Planning and Environment's [State Significant Infrastructure and State Significant Project Guidelines \(DPIE, 2021a\)](#).

This report identifies the issues raised during exhibition of the environmental impact statement and provides responses to those issues. It includes information regarding additional studies carried out since the exhibition of the EIS, identifies project amendments and refinements, clarifies certain information included in the EIS, and provides updated mitigation measures in response to the submissions.

The structure of the report is outlined in Table 1.1.

Table 1.1 Structure of this report

Chapter	Description
Chapter 1	Introduction and background (this chapter) Includes a summary of the project and the assessment undertaken to date.
Chapter 2	Stakeholder and community engagement Outlines the stakeholder and community engagement process for the exhibition of the EIS and during preparation of this report.
Chapter 3	Analysis of submissions Analyses the submissions received during public exhibition of the EIS.
Chapter 4	Actions taken since exhibition Summarises the actions taken by Transport since the public exhibition of the EIS, including additional assessments. Provides an outline of the proposed amendments and refinements to the project, and clarifies certain information included in the EIS in response to issues raised in the submissions and during ongoing stakeholder and community engagement.
Chapter 5	Response to NSW Government agency submissions and advice Provides a summary of issues raised in submissions and advice received from NSW Government agencies and Transport's responses.
Chapter 6	Response to council submissions Provides a summary of issues raised in the council submissions and Transport's responses.
Chapter 7	Response to key organisations Provides a summary of issues raised in the submissions from key organisations and Transport's responses.
Chapter 8	Response to community submissions Provides a summary of issues raised in submissions from members of the public and other organisations and Transport's responses.
Chapter 9	Conclusion and next steps Provides a conclusion to the report.
Chapter 10	References